

## Addendum No. 1

---

Owner: Gerald R. Ford International Airport Authority

---

Project Title: Runway 8R Wildlife Habitat Mitigation

---

Project #: 2230912

Date of Addendum: November 27, 2024

---

# Notice to Prospective Bidders

In accordance with Article 7 of the Agreement, this Addendum is hereby issued as part of the Contract Documents.

### General Information

1. The Prebid meeting will be held in the Golden Eagle Room on the 2<sup>nd</sup> floor of the Gerald R. Ford International Airport Terminal building on December 4, 2024 at 10:00 am.
2. The engineer will perform soil borings over the surcharge limits identified on Plan Sheet C-100 to verify peat removal. Contractor shall provide access to the surcharge area, suitable for an off-road boring rig. Should the contractor elect to surcharge areas outside those which are identified on Sheet C-100, the contractor will be responsible for performing soil borings at a frequency of one (1) boring for every 300 square yards to a depth of 3' below the bottom of the peat/organics if such soils are still present after surcharging operations.

### Plans

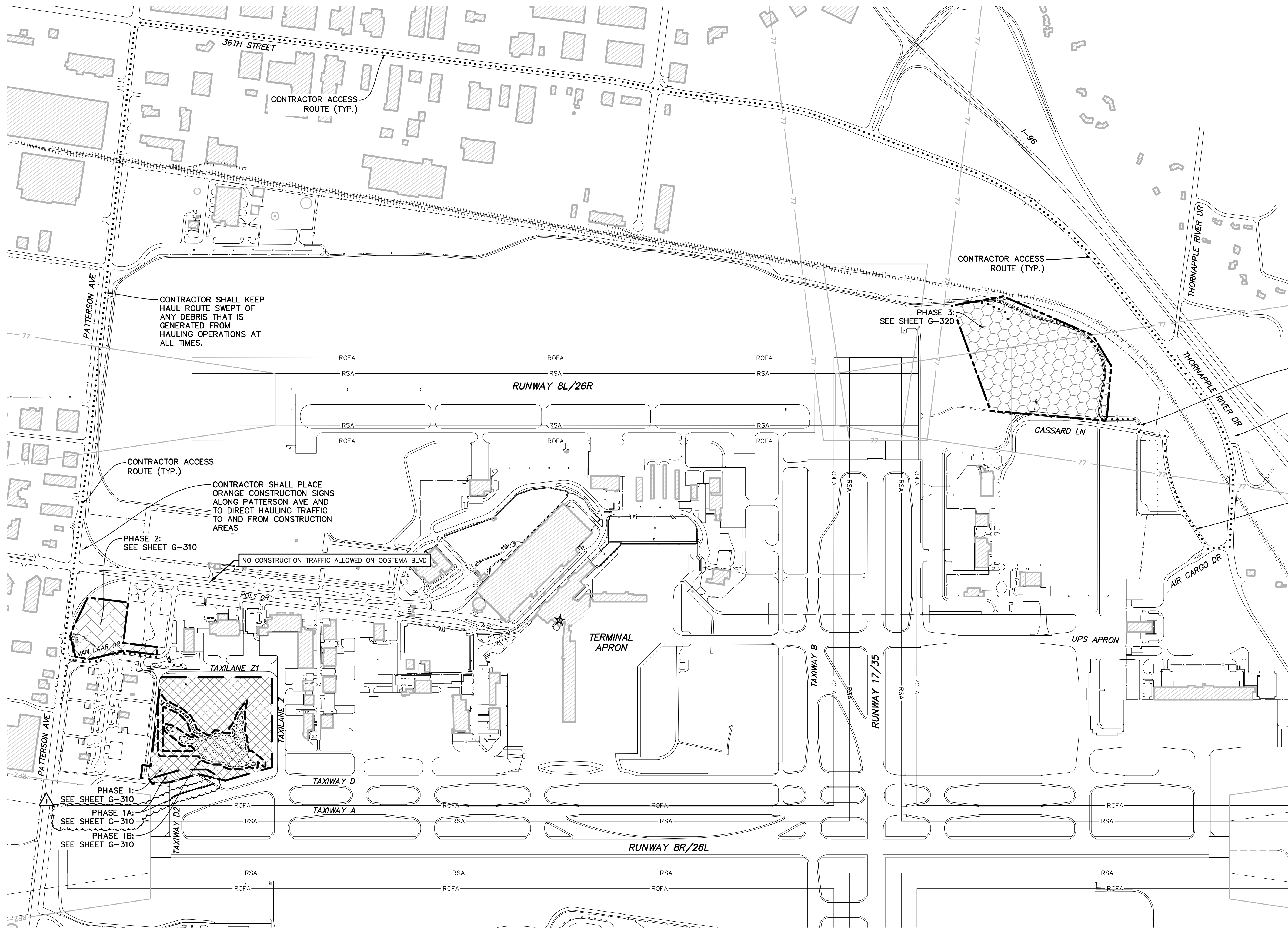
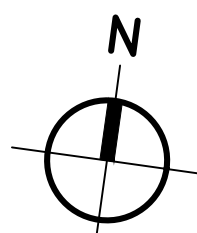
1. **REISSUE** Sheet G-300. Low profile barricade locations were clarified on this sheet.
2. **REISSUE** Sheet G-310. Contractor shall cover the existing taxiway edge lights along the edge of the hold apron that is closed for Phase 1A. Safety and Phasing Notes have been updated as shown on the enclosed sheet.
3. **REISSUE** Sheet G-320. Safety and Phasing Notes have been updated as shown on the enclosed sheet.
4. **REISSUE** Sheet G-350. Safety and Operations Notes have been updated as shown on the enclosed sheet.



Brian J. Merk

Enclosures: Plan Sheets G-300, G-310, G-320, G-350

c: Plan Holders



CONTRACTOR SHALL KEEP HAUL ROUTE SWEEPED OF ANY DEBRIS THAT IS GENERATED FROM HAULING OPERATIONS AT ALL TIMES.

CONTRACTOR SHALL PLACE ORANGE CONSTRUCTION SIGNS ALONG PATTERSON AVE AND TO DIRECT HAULING TRAFFIC TO AND FROM CONSTRUCTION AREAS

PHASE 3: SEE SHEET G-320

GATE 80, CONTRACTOR TO COORDINATE WITH AIRPORT SECURITY FOR HAULING SPOILS.

CONTRACTOR SHALL PLACE ORANGE CONSTRUCTION SIGNS ALONG THORNAPPLE RIVER DR AND AIR CARGO DR TO DIRECT HAULING TRAFFIC TO AND FROM CONSTRUCTION AREAS

CASSARD LANE AND PERIMETER ROAD (AND THORNAPPLE RIVER DRIVE) TO REMAIN FREE OF DEBRIS AT ALL TIMES. TO BE SWEEPED AS NEEDED.

NO OFF-ROAD VEHICLES ALLOWED ON CASSARD LANE OR PERIMETER ROAD OR THORNAPPLE RIVER DRIVE.

NO CONSTRUCTION TRAFFIC ALLOWED ON OOSTEMA BLVD

PHASE 1: SEE SHEET G-310  
PHASE 1A: SEE SHEET G-310  
PHASE 1B: SEE SHEET G-310

**LEGEND**

	PHASE 1 CONSTRUCTION AREA
	PHASE 1A CONSTRUCTION AREA
	PHASE 1B CONSTRUCTION AREA
	PHASE 2 CONSTRUCTION AREA
	PHASE 3 CONSTRUCTION AREA
	ACCESS ROUTE
	ROFA RUNWAY OBJECT FREE AREA (ROFA)
	RPZ RUNWAY PROTECTION ZONE (RPZ)
	RSA RUNWAY SAFETY AREA (RSA)
	ROFZ RUNWAY OBJECT FREE ZONE (ROFZ)
	TOFA TAXIWAY OBJECT FREE AREA (TOFA)
	TSA TAXIWAY SAFETY AREA (TSA)
	BRL BUILDING RESTRICTION LINE
	1013 PART 77 SURFACE
	LOW PROFILE BARRICADE
	PROJECT COORDINATE LOCATION
	BARRICADE PLACEMENT (BY PHASE)
	EXISTING PAVEMENT
	EXISTING BUILDING

**811**  
UTILITY LOCATIONS ARE DERIVED FROM ACTUAL MEASUREMENTS OR AVAILABLE RECORDS. THEY SHOULD NOT BE INTERPRETED TO BE EXACT LOCATIONS NOR SHOULD IT BE ASSUMED THAT THEY ARE THE ONLY UTILITIES IN THIS AREA.  
Know what's below.  
Call before you dig.

NO.	REVISIONS	BY	DATE	DRAWN
1	ADDENDUM No.1	C.M.S.	11.27.24	STAFF
				DATE NOV. '24
				CHECKED B.J.M.
				DATE NOV. '24

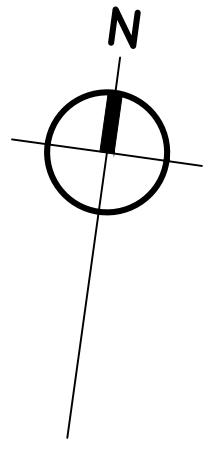
**Prein & Newhof**  
Engineers • Surveyors • Environmental • Laboratory

GERALD R. FORD INTERNATIONAL AIRPORT  
KENT COUNTY, MICHIGAN  
RUNWAY 8R WILDLIFE HABITAT MITIGATION  
OVERALL SAFETY AND PHASING PLAN

PROJECT NO.  
**2230912**  
SHEET NO.  
**G-300**

C:\USERS\CSCHREIBER\APPDATA\LOCAL\TEMP\ACPIBUBISHI\_20952\2230912\_G-300\_CSPP.DWG - Nov. 27 2024 - 09:30am - Prein\811.mxd



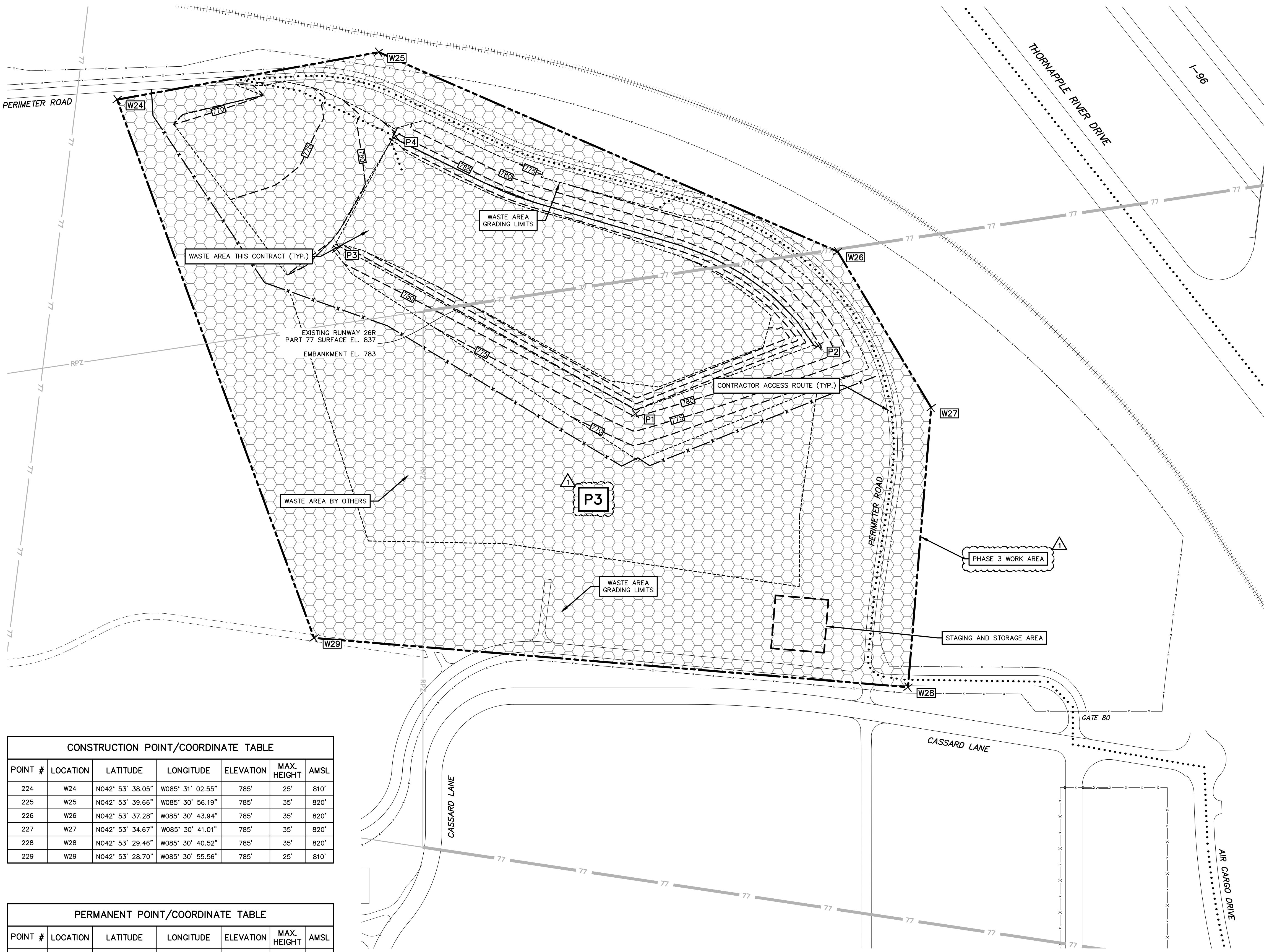


**SAFETY AND PHASING NOTES**

- GENERAL NOTES**
1. ALL PERSONNEL PERFORMING WORK INSIDE THE AOA FENCE SHALL BE BADGED IN ACCORDANCE WITH AIRPORT SECURITY REGULATIONS.
  2. REFER TO SHEET G-350 FOR ADDITIONAL SAFETY AND SECURITY REQUIREMENTS.
  3. FOR ALL PHASES HEREIN, CONTRACTOR'S EQUIPMENT MUST ENTER THE WORK AREAS THROUGH THE ACCESS ROUTES SHOWN.
  4. CONTRACTOR SHALL PROVIDE, PLACE, AND MAINTAIN LOW PROFILE BARRICADES AND MDOT CHANNELIZING DEVICES AT THE LOCATIONS SHOWN IMMEDIATELY UPON CLOSING THE WORK AREA.
  5. MAXIMUM EQUIPMENT HEIGHT FOR ANY PHASE IS AS SHOWN ON THE CONSTRUCTION COORDINATE TABLE THIS SHEET.
  6. THE CONTRACTOR SHALL ACCESS WASTE AREA THROUGH GATE 80.
  7. CONTRACTOR MAY USE ANY AND ALL OF THE CONSTRUCTION AREA FOR STAGING AND STORAGE, IN ADDITION TO THE STAGING AND STORAGE AREAS SHOWN ON THIS SHEET. MAX EQUIPMENT HEIGHT SHALL BE AS SHOWN ON THE COORDINATE TABLE(S) THIS SHEET.
  8. SHOULD THE CONTRACTOR FAIL TO COMPLETE ANY AND ALL PHASES OF WORK WITHIN THE CALENDAR DAYS ALLOTTED, OR CALENDAR DATE SPECIFIED, LIQUIDATED DAMAGES SHALL BE ASSESSED AT A RATE OF \$2,000 PER DAY UNTIL COMPLETION.
  9. REFER TO SHEET G-350 FOR WORK ZONE LIGHTING REQUIREMENTS DURING NIGHTTIME CONSTRUCTION.
- PHASE 3**
10. THIS PHASE WILL INCLUDE HAULING EXCESS SPOILS FROM PHASE 1 & 1A AREA, MASS GRADING, AND RESTORATION PER PLANS WITHIN THE WORK AREA SHOWN.
  11. THIS PHASE SHALL BE COMPLETED WITHIN 150 CALENDAR DAYS AND WILL RUN CONCURRENTLY WITH PHASE 1.
  12. NO LOW PROFILE BARRICADES NOR MDOT CHANNELIZING DEVICES ANTICIPATED THIS PHASE.

**LEGEND**

	PHASE 2 CONSTRUCTION AREA
	STAGING & STORAGE AREA
	ACCESS ROUTE
	ROFA - RUNWAY OBJECT FREE AREA (ROFA)
	RPZ - RUNWAY PROTECTION ZONE (RPZ)
	RSA - RUNWAY SAFETY AREA (RSA)
	ROFZ - RUNWAY OBJECT FREE ZONE (ROFZ)
	TOFA - TAXIWAY OBJECT FREE AREA (TOFA)
	TSA - TAXIWAY SAFETY AREA (TSA)
	BRL - BUILDING RESTRICTION LINE (64')
	850 - PART 77 SURFACE
	CONSTRUCTION SAFETY FENCE
	LOW PROFILE BARRICADE
	PROJECT COORDINATE LOCATION
	BARRICADE PLACEMENT (BY PHASE)
	EXISTING PAVEMENT
	EXISTING BUILDING



**CONSTRUCTION POINT/COORDINATE TABLE**

POINT #	LOCATION	LATITUDE	LONGITUDE	ELEVATION	MAX. HEIGHT	AMSL
224	W24	N042° 53' 38.05"	W085° 31' 02.55"	785'	25'	810'
225	W25	N042° 53' 39.66"	W085° 30' 56.19"	785'	35'	820'
226	W26	N042° 53' 37.28"	W085° 30' 43.94"	785'	35'	820'
227	W27	N042° 53' 34.67"	W085° 30' 41.01"	785'	35'	820'
228	W28	N042° 53' 29.46"	W085° 30' 40.52"	785'	35'	820'
229	W29	N042° 53' 28.70"	W085° 30' 55.56"	785'	25'	810'

**PERMANENT POINT/COORDINATE TABLE**

POINT #	LOCATION	LATITUDE	LONGITUDE	ELEVATION	MAX. HEIGHT	AMSL
1001	P1	N042° 53' 33.73"	W085° 30' 48.38"	783'	0'	783'
1002	P2	N042° 53' 35.49"	W085° 30' 44.07"	785'	0'	785'
1003	P3	N042° 53' 35.91"	W085° 30' 56.48"	783'	0'	783'
1004	P4	N042° 53' 38.15"	W085° 30' 55.43"	785'	0'	785'

**811**  
 UTILITY LOCATIONS ARE DERIVED FROM ACTUAL MEASUREMENTS OR AVAILABLE RECORDS. THEY SHOULD NOT BE INTERPRETED TO BE EXACT LOCATIONS NOR SHOULD IT BE ASSUMED THAT THEY ARE THE ONLY UTILITIES IN THIS AREA.  
 Know what's below. Call before you dig.

NO.	REVISIONS	BY	DATE	DRAWN STAFF
1	ADDENDUM No.1	C.M.S.	11.27.24	STAFF
				DATE NOV. '24
				CHECKED B.J.M.
				DATE NOV. '24

**Prein & Newhof**  
 Engineers • Surveyors • Environmental • Laboratory

GERALD R. FORD INTERNATIONAL AIRPORT  
 KENT COUNTY, MICHIGAN  
**RUNWAY 8R WILDLIFE HABITAT MITIGATION**  
**PHASE 3**

PROJECT NO.  
**2230912**  
 SHEET NO.  
**G-320**

CONTRACT TIME

1. THE CONTRACT SHALL BE COMPLETED WITHIN 150 CALENDAR DAYS OF THE NOTICE TO PROCEED. SHOULD THE CONTRACTOR FAIL TO COMPLETE ALL WORK INCLUDING "PUNCH LIST" WITHIN THAT TIME, LIQUIDATED DAMAGES SHALL BE ASSESSED, AT THE RATE SPECIFIED IN THE AGREEMENT BETWEEN OWNER AND CONTRACTOR, PER CALENDAR DAY UNTIL COMPLETION. THE "NOTICE TO PROCEED" IS ANTICIPATED TO BE ISSUED IN EARLY 2025.

GENERAL CONSIDERATIONS

2. THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION MEETING. AIRPORT OPERATIONAL SAFETY AND PHASING WILL BE A KEY ELEMENT DISCUSSED. THE CONTRACTOR SHALL PRESENT A CONSTRUCTION SCHEDULE AT THE PRECONSTRUCTION MEETING. CHANGES TO THE CONSTRUCTION SCHEDULE, OR REQUESTS FOR CHANGE TO THE PHASING PLAN, MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL AT LEAST THREE WORKING DAYS PRIOR TO CHANGE. [2.5]

3. THE CONTRACTOR SHALL PERFORM ALL WORK IN ACCORDANCE WITH THE SAFETY AND PHASING PLAN. ALL WORK SHALL BE PHASED AND COMPLETED IN ACCORDANCE WITH THE PHASING PLANS. THE CONTRACTOR MUST MAINTAIN OPERATIONAL SAFETY ON THE AIRPORT DURING CONSTRUCTION IN ACCORDANCE WITH FAA AC 150/5370-2G, AND IN ACCORDANCE WITH THE CONTRACTOR'S SUBMITTED SAFETY PLAN COMPLIANCE DOCUMENT (SPCD). THE SPCD SHALL BE SUBMITTED AT THE PRE-CONSTRUCTION MEETING. [2.5]

4. THE CONTRACTOR SHALL HAVE A SUPERINTENDENT ON SITE AT ALL TIMES WHILE CONSTRUCTION IS TAKING PLACE. THE CONTRACTOR'S SUPERINTENDENT SHALL ATTEND A CONSTRUCTION PROGRESS AND SAFETY/SECURITY MEETING ON A WEEKLY BASIS THROUGHOUT THE DURATION OF THE PROJECT OR AS REASONABLY REQUESTED BY THE OWNER. THE MEETINGS WILL BE HELD AT A LOCATION ON AIRPORT PROPERTY TO BE DESIGNATED BY THE OWNER. OPERATIONAL SAFETY WILL BE A STANDING AGENDA ITEM FOR THESE MEETINGS. [2.5]

5. SHOULD CHANGES IN THE SCOPE OR DURATION OF THE PROJECT OCCUR MAKING REVISIONS TO THE CONSTRUCTION SAFETY PHASING PLAN (CSPP) NECESSARY, THE REVISED CSPP MAY BE REVIEWED AND APPROVED BY THE AIRPORT AND THE FAA PRIOR TO ISSUING A CONTRACT MODIFICATION. [2.5]

6. ALL PHASES AND CLOSURES SHALL BE COORDINATED WITH AIRPORT OPERATIONS AT LEAST 24 HOURS IN ADVANCE. AIRPORT OPERATIONS WILL COORDINATE NOTAMS AND CLOSURES WITH LOCAL FAA STAFF TO NOTIFY AIRPORT USERS OF CONSTRUCTION OR MAINTENANCE ACTIVITY. [2.6, 2.22.1.2, 2.22.3.2]

7. CONTRACTOR SHALL PROVIDE AN UPDATED PROGRESS SCHEDULE WITH EACH APPLICATION FOR PARTIAL PAYMENT. [2.6]

8. THE CONTRACTOR SHALL MAKE ALL INQUIRIES NECESSARY SO THAT THEIR PROPOSAL SHALL BE BASED UPON FULL KNOWLEDGE AND ESTIMATION OF THE CONDITIONS AND REQUIREMENTS TO BE MET.

9. FAA ATO/OPERATIONS CONTACT IS PETER WILKE AT 616-464-5371. THE CONTRACTOR SHALL COORDINATE ANY CSPP CHANGES WITH THE FAA THROUGH THE AIRPORT. THIS INFORMATION IS PROVIDED FOR EMERGENCY USE ONLY. [2.6, 2.13.1]

10. TWO-WAY RADIO COMMUNICATIONS SHALL ONLY BE PERFORMED BY CONTRACTOR STAFF WHO HAVE BEEN TRAINED TO DO SO BY AIRPORT OPERATIONS STAFF. [2.9.2.9]

11. ALL VEHICLE OPERATIONS WITHIN THE AIRPORT PERIMETER SHALL BE CONDUCTED WITH CAUTION AND WHILE MONITORING AND OPERATING AT 121.8 MHZ. AIRCRAFT SHALL HAVE THE RIGHT-OF-WAY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MONITOR 121.8 MHZ AND ENSURE AIR OPERATION AREAS (AOA) ARE CLEAR PRIOR TO CONTRACTOR VEHICLES OPERATING IN THOSE AREAS. RADIOS ARE NOT AVAILABLE AT THE AIRPORT. COST OF FURNISHING AND MAINTAINING RADIOS SHALL BE INCLUDED IN LUMP SUM ITEM "SAFETY AND SECURITY". INADVERTENT INCURSION ONTO OR ACROSS AN ACTIVE RUNWAY OR ITS APPROACHES SHALL SUBJECT THE CONTRACTOR TO A FINE OF \$10,000 PER OCCURRENCE. [2.9.2.9]

12. THE CONTRACTOR SHALL PERFORM ALL WORK IN ACCORDANCE WITH THE SAFETY AND PHASING PLAN. ALL WORK SHALL BE PHASED AND COMPLETED IN ACCORDANCE WITH THE PHASING PLANS. THE CONTRACTOR MUST MAINTAIN OPERATIONAL SAFETY ON THE AIRPORT DURING CONSTRUCTION IN ACCORDANCE WITH FAA AC 150/5370-2G, AND IN ACCORDANCE WITH THE CONTRACTOR'S SUBMITTED SAFETY PLAN COMPLIANCE DOCUMENT (SPCD). THE SPCD SHALL BE SUBMITTED AT THE PRE-CONSTRUCTION MEETING.

13. CONTRACTOR SHALL IMMEDIATELY NOTIFY THE AIRPORT OF ANY CONDITION THAT MAY, IN THE CONTRACTOR'S OPINION, ADVERSELY AFFECT OPERATIONAL SAFETY.

NAVAIDS

14. CONTRACTOR SHALL NOT WORK INSIDE NAVIGATIONAL AID (NAVAIDS) CRITICAL AREAS WITHOUT CLEARANCE FROM AIRPORT OPERATIONS. OPERATIONS WILL COORDINATE NOTAMS AND CLOSURES WITH THE LOCAL FEDERAL AVIATION ADMINISTRATION (FAA) STAFF. CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO PROTECT NAVAIDS, LIGHTS AND SIGNS FROM DAMAGE. DAMAGE TO ANY NAVAID EQUIPMENT OR UNDERGROUND UTILITY SHALL BE THE CONTRACTOR'S RESPONSIBILITY AND SHALL BE FIXED, REPLACED OR REPAIRED AS DIRECTED BY THE ENGINEER AT NO COST TO THE OWNER. CONTRACTOR SHALL PROVIDE TEMPORARY POWER TO FAA EQUIPMENT IF POWER LOSS RESULTS FROM CONSTRUCTION OPERATIONS. [2.8]

CONTRACTOR ACCESS

15. STOCKPILES SHALL BE IN LOCATION AND HEIGHT AS DESIGNATED ON THE PLANS. [2.9.1]

16. THE CONTRACTOR SHALL LIMIT ACCESS INTO THE AIRPORT TO AUTHORIZED PERSONNEL ONLY. THE CONTRACTOR SHALL PROVIDE TRAINED PERSONNEL AT ACCESS GATES WHENEVER THOSE GATES ARE IN USE BY CONSTRUCTION PERSONNEL. IF A GATE ATTENDANT IS USED, THE CONTRACTOR WILL BE REQUIRED TO LOG THE ENTRANCE INTO AND EXIT FROM THE CONSTRUCTION AREA OF ALL CONSTRUCTION VEHICLES. VEHICLE TRAFFIC TO BE LOADED SHALL INCLUDE, BUT NOT BE LIMITED TO DELIVERY VEHICLES, CONSTRUCTION OBSERVER, CONTRACTOR'S VEHICLES, AND OTHER CONSTRUCTION VEHICLES. ACCESS THROUGH SECURE GATES WILL BE COORDINATED WITH THE AIRPORT. KEYS AND/OR CARDS WILL BE ISSUED FOR CONSTRUCTION ACCESS. NO CONTRACTOR LOCKS WILL BE ALLOWED. [2.9.1]

17. CONTRACTOR SHALL LIMIT HAULING OF MATERIAL AND EQUIPMENT TO THOSE ROUTES INDICATED ON THE PLANS. CONTRACTOR SHALL GIVE RIGHT-OF-WAY TO AIRPORT EMERGENCY VEHICLES AT ALL TIMES. [2.9.2.3]

18. ENTRANCE OF MULTIPLE CONSTRUCTION VEHICLES INTO THE AOA SHALL ONLY BE ACCOMPLISHED BY AIRPORT APPROVED ESCORT PROCEDURES. [2.9.2.6]

19. ALL CONSTRUCTION EMPLOYEES REQUIRED TO DRIVE IN THE AOA SHALL BE TRAINED FOR RAMP DRIVING PRIVILEGES PRIOR TO STARTING WORK. TRAINING SHALL INCLUDE INSTRUCTIONS FOR ESCORTING OTHERS IN THE AOA. [2.9.2.5]

20. ALL PERSONNEL WORKING IN THE AIRPORT OPERATIONS AREA SHALL EITHER BE ESCORTED TRAINED PERSONNEL OR ATTEND TRAINING PROVIDED BY THE AIRPORT. [2.9.2.6, 2.9.2.7]

21. WHEN NOT IN USE AND DURING NON-WORKING HOURS, CONTRACTOR'S EQUIPMENT SHALL BE PARKED WITHIN THE CONTRACTOR'S STAGING AND STORAGE AREA. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE SAFETY AND SECURITY OF HIS/HER STORAGE AREA AND ALL CONSTRUCTION AREAS. THE CONTRACTOR SHALL CONFINE THE STOCKPILING OF MATERIAL TO THE STAGING AND STORAGE AREA. PARKING OF EMPLOYEE'S VEHICLES SHALL IN THE STAGING AREA(S) SPECIFIED. [2.9.2.2, 2.22.2]

22. CONTRACTOR ACCESS PROCEDURES SPECIFIED ABOVE SHALL ALSO APPLY TO ALL PEDESTRIAN OPERATIONS INTO THE AOA. [2.9.2]

WILDLIFE MANAGEMENT

23. CONTRACTOR SHALL COLLECT AND DISPOSE OF ALL LUNCH WRAPPERS, FOOD AND OTHER WILDLIFE ATTRACTANTS AS NOTED IN AIRPORT RULES AND REGULATIONS. CONTRACTOR PERSONNEL SHALL BE AWARE OF AND AVOID CONSTRUCTION ACTIVITIES THAT CAN CREATE WILDLIFE HAZARDS ON AIRPORTS INCLUDING CREATING AREAS OF STANDING WATER AND TALL GRASS. CONTRACTOR SHALL NOTIFY THE AIRPORT IF DEER, BIRDS, OR OTHER WILDLIFE ARE OBSERVED INSIDE THE AIRPORT PERIMETER FENCE. [2.10]

24. CONTRACTOR SHALL NOTIFY THE AIRPORT IF DEER, BIRDS, OR OTHER WILDLIFE IS OBSERVED IN THE WETLAND AREA DURING HABITAT DISRUPTION. [2.10.5]

FOREIGN OBJECT DEBRIS MANAGEMENT

25. MATERIAL TRACKED OR OTHERWISE DEPOSITED ON ACTIVE PAVEMENTS BY CONSTRUCTION OPERATIONS SHALL BE REMOVED IMMEDIATELY IN ORDER TO PREVENT DAMAGE TO AIRCRAFT. ALL FOREIGN OBJECT DEBRIS (FOD) FOUND ON AIRCRAFT PAVEMENTS AND ADJACENT TO WORK AREAS SHALL BE SUFFICIENT CAUSE FOR THE OWNER TO STOP WORK. SAID OBJECTS SHALL BE REMOVED

IMMEDIATELY AND PAVEMENTS SWEEPED OR VACUUMED PRIOR TO WORK RESTARTING. [2.11]

26. THE CONTRACTOR SHALL CONTROL DUST FROM CONTRACTOR WORK AREAS BY VACUUM TYPE SWEEPING, WATERING OR OTHER METHODS AS APPROVED BY THE ENGINEER. DUST CONTROL WILL BE PAID FOR UNDER THE LUMP SUM ITEM "SAFETY AND SECURITY". [2.11]

HAZARDOUS MATERIAL MANAGEMENT

27. CONTRACTOR SHALL UNDERSTAND AIRPORT OPERATIONS HAZARDOUS MATERIALS MANAGEMENT PROCEDURES AND REPORT ANY SUSPICIOUS MATERIAL TO OPERATIONS IMMEDIATELY. [2.12]

28. CONTRACTOR SHALL BE RESPONSIBLE FOR HANDLING AND CONTAINING FUEL, OIL AND OTHER HAZARDOUS MATERIALS DELIVERED TO THE AIRPORT FOR CONTRACTOR OPERATIONS. MATERIALS SHALL BE HANDLED IN ACCORDANCE WITH THE MICHIGAN ENVIRONMENT, GREAT LAKES, AND ENERGY (EGLE), THE MICHIGAN OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (MIOSHA), THE ENVIRONMENTAL PROTECTION AGENCY (EPA), OR OTHER AGENCIES WITH REGULATORY AUTHORITY OVER THE ASSOCIATED MATERIAL. ALL RELEASES OF CONTRACTOR'S MATERIALS SHALL BE REPORTED TO THE AIRPORT IMMEDIATELY AND CLEANED UP BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CLEANUP COSTS INCLUDING FINES AND PENALTIES ASSESSED TO THE AIRPORT AS A RESULT OF THE RELEASE. CONTRACTOR SHALL SUBMIT COPIES OF ALL MATERIAL SAFETY DATA SHEETS ASSOCIATED WITH THE WORK TO THE ENGINEER PRIOR TO DELIVERY TO THE SITE. CONTRACTOR SHALL NOTIFY ARFF OF LOCATIONS OF ANY ON-SITE FUEL TANKS NEEDED FOR CONSTRUCTION ACTIVITIES. [2.12]

NOTIFICATION OF CONSTRUCTION ACTIVITIES

29. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH A NAME AND TELEPHONE NUMBER OF TWO (2) INDIVIDUALS THAT WILL BE AVAILABLE 24 HOURS A DAY, SEVEN DAYS A WEEK, FOR USE IN EMERGENCIES. THESE CONTACT PERSONS SHALL HAVE THE AUTHORITY TO IMMEDIATELY CORRECT ANY SAFETY OR SECURITY DEFICIENCY CAUSED BY THE CONTRACTOR OR THE CONTRACTOR'S ACTIVITIES, AS IDENTIFIED BY THE OWNER OR THE ENGINEER. [2.13.1, 2.20.2.7]

30. A FULL LIST OF RESPONSIBLE REPRESENTATIVES WILL BE PROVIDED AT THE PRE-CONSTRUCTION MEETING. CONTRACTOR SHOULD CALL 911 FOR MEDICAL, FIRE, AND POLICE RESPONSE. IN CASE OF AN EMERGENCY, THE CONTRACTOR SHALL MAKE EVERY EFFORT TO REMOVE THE EQUIPMENT AND PERSONNEL FROM THE AFFECTED AREA AND ABIDE BY ANY DECISIONS CONCERNING EQUIPMENT AND PERSONNEL MADE BY THE OWNER OR ENGINEER. THE CONTRACTOR SHALL ALSO IMMEDIATELY NOTIFY AIRPORT OPERATIONS AT 616-233-6055. [2.13.1, 2.13.2, 2.13.3]

31. CONTRACTOR SHALL IMMEDIATELY NOTIFY AIRPORT OPERATIONS OF ANY CONDITION THAT MAY, IN THE CONTRACTOR'S OPINION, ADVERSELY AFFECT OPERATIONAL SAFETY. [2.13]

32. EMERGENCY, ARFF AND OTHER AIRPORT VEHICLES ARE NOT ANTICIPATED TO BE DETOURED DUE TO THIS CONSTRUCTION PROJECT. EMERGENCY VEHICLES SHALL HAVE THE RIGHT-OF-WAY AT ALL TIMES.

33. THE AIRPORT WILL COORDINATE NOTAMS AND CLOSURES. [2.13.2]

INSPECTION REQUIREMENTS

34. INSPECTIONS/OBSERVATIONS WILL BE EXECUTED BY THE ENGINEER OR ENGINEER'S REPRESENTATIVE AS WELL AS THE AIRPORT. ULTIMATELY THE CONTRACTOR IS RESPONSIBLE FOR INSPECTING THEIR OWN WORK AND VERIFYING THAT THE WORK IS BEING COMPLETED PER THE CONTRACT DOCUMENTS. [2.14.1, 2.14.2]

35. INSPECTION OF THE SITE SHALL BE DONE ON A DAILY BASIS. [2.14.1]

36. FINAL INSPECTIONS WILL BE CONDUCTED BY THE AIRPORT PERSONNEL AND THE OWNER/ENGINEER. FINAL INSPECTIONS WILL NOT BE CONDUCTED UNTIL THE CONTRACTOR ASSURES THAT ALL WORK HAS BEEN COMPLETED PER THE CONTRACT DOCUMENTS. [2.14.3]

UNDERGROUND UTILITIES

37. PRIOR TO EXCAVATION, EFFORT SHALL BE MADE TO DETERMINE WHETHER UNDERGROUND SEWER, WATER, FUEL, ELECTRIC LINES, ETC., WILL BE ENCOUNTERED, AND IF SO, WHERE SUCH UNDERGROUND INSTALLATIONS ARE LOCATED. WHEN THE EXCAVATION APPROACHES THE APPROXIMATE LOCATIONS OF SUCH AN INSTALLATION, THE EXACT LOCATIONS SHALL BE DETERMINED BY CAREFUL PROBING OR HAND DIGGING, AND WHEN IT IS

UNCOVERED, ADEQUATE PROTECTION SHALL BE PROVIDED FOR THE EXISTING INSTALLATION. ALL KNOWN OWNERS OF UNDERGROUND FACILITIES IN THE AREA CONCERNED SHALL BE ADVISED OF PROPOSED WORK AT LEAST 48 HOURS PRIOR TO THE START OF ACTUAL EXCAVATION. REFER TO SECTIONS 70-10, 70-11 AND 70-15 OF THE GENERAL PROVISIONS FOR CONSTRUCTION OF AIRPORTS FOR A DESCRIPTION OF THE CONTRACTOR'S ASSUMPTION OF RISK AND RESPONSIBILITY TO PROTECT PROPERTY OF OTHERS DURING CONSTRUCTION. [2.15]

38. EXISTING UTILITIES WERE TAKEN FROM PLANS OF RECORD. THEY HAVE BEEN SHOWN TO THE EXTENT KNOWN AND ARE OFFERED IN GOOD FAITH SOLELY FOR INFORMATIONAL PURPOSES. THEY MAY NOT REFLECT ACTUAL LOCATIONS AND MAY NOT BE ALL INCLUSIVE. [2.15]

39. CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING MISSDIG PRIOR TO STARTING WORK. CONTRACTOR SHALL VERIFY ALL UTILITY LOCATIONS PRIOR TO DEMOLITION AND CONSTRUCTION. [2.15]

40. MISSDIG AND OTHER UTILITY MARKING SERVICES DO NOT LOCATE FAA ATO/TECHNICAL OPERATIONS. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH FAA ATO/TECHNICAL OPERATIONS PRIOR TO STARTING WORK.

41. CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO PROTECT UNDERGROUND UTILITIES. DAMAGE TO ANY EQUIPMENT OR UNDERGROUND UTILITY SHALL BE THE CONTRACTOR'S RESPONSIBILITY AND SHALL BE FIXED, REPLACED OR REPAIRED IMMEDIATELY AS DIRECTED BY THE ENGINEER AT NO COST TO THE OWNER. [2.15]

PENALTIES

42. IF THE CONTRACTOR FAILS TO COMPLY WITH THE AIRPORT RULES AND REGULATIONS AND THE CSPP, CONSTRUCTION WILL BE SUSPENDED UNTIL THE CONTRACTOR DEMONSTRATES PROCEDURES ARE IN PLACE TO COMPLY AND THE MODIFIED PROCEDURES ARE ACCEPTED BY THE AIRPORT. FAILURE TO COMPLETE THE PROJECT ON TIME DUE TO THE CONTRACTOR'S INABILITY TO COMPLY WITH THE REQUIREMENTS NOTED HEREIN WILL NOT BE CONSIDERED AS A BASIS FOR ADDITIONAL TIME. INDIVIDUAL EMPLOYEES WHO DO NOT CONFORM AS NOTED HEREIN MAY BE FINED, SUSPENDED OR DENIED ACCESS TO THE AIRPORT. IF A VEHICLE IS INVOLVED IN A RUNWAY INCURSION, THE OPERATOR MAY HAVE DRIVING PRIVILEGES AND ACCESS TO THE AOA REVOKED. [2.16]

43. FAILURE TO COMPLY WITH SECURITY REQUIREMENTS WILL RESULT IN SUSPENSION OF THE CONTRACTOR'S OPERATIONS UNTIL A PLAN TO CORRECT THE DEFICIENCIES IS APPROVED BY THE ENGINEER AND OWNER. THIS SUSPENSION SHALL NOT BE CAUSE FOR ADDITIONAL CONTRACT TIME. [2.16]

44. THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR ANY FINES LEVIED AGAINST THE AIRPORT OR BY THE AIRPORT FOR SECURITY VIOLATIONS WHICH ARE A RESULT OF THE CONTRACTOR'S FAILURE TO COMPLY WITH SECURITY REGULATIONS. SUCH FINES SHALL BE DEDUCTED FROM THE CONTRACT BY CHANGE ORDER. [2.16]

45. ALL DELIVERIES SHALL BE MADE TO THE CONTRACTOR'S STORAGE AREA. THE AIRPORT RESERVES THE RIGHT TO DEDUCT FROM THE CONTRACT TIME SPENT BY AIRPORT, TENANT, VENDOR OR CONSULTANT STAFF TO DIRECT DELIVERIES TO THE APPROPRIATE AREA. TIME WILL BE CHARGED AT A RATE OF \$75 PER HOUR OR ESTABLISHED CONTRACTUAL RATES, WHICHEVER IS GREATER. TIME WILL BE CHARGED IN ONE HOUR INCREMENTS.

46. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO PROTECT EXISTING BUILDINGS AND INFRASTRUCTURE FROM DAMAGE. DAMAGE TO ANY EXISTING INFRASTRUCTURE SHALL BE THE CONTRACTOR'S RESPONSIBILITY AND SHALL BE FIXED, REPLACED OR REPAIRED AS DIRECTED BY THE ENGINEER AT NO COST TO THE OWNER. [2.16]

SPECIAL CONDITIONS

47. SHOULD SUSPENSION OF CONSTRUCTION BECOME NECESSARY DUE TO LOW-VISIBILITY OPERATIONS, SNOW REMOVAL, AIRCRAFT IN DISTRESS, AIRCRAFT ACCIDENT, SECURITY BREACH, VEHICLE / PEDESTRIAN DEVIATION (VPD) OR OTHER CONDITIONS, THE AIRPORT RESERVES THE RIGHT TO NOTIFY THE CONTRACTOR AND ISSUE SPECIAL INSTRUCTIONS AS NECESSARY FOR THE SITUATION. [2.17]

RUNWAY AND TAXIWAY VISUAL AIDS

48. THROUGHOUT THE DURATION OF THE CONSTRUCTION PROJECT, CONTRACTOR SHALL VERIFY THAT MARKING, LIGHTING, SIGNS, AND VISUAL NAVAIDS REMAIN IN PLACE AND OPERATIONAL AND CLEARLY MARKED AND VISIBLE AT ALL TIMES. CONTRACTOR TO COORDINATE WITH THE AIRPORT TO HAVE APPLICABLE LIGHTING SYSTEMS DE-ENERGIZED

DURING AOA CLOSURE (S). [2.18.1]

49. RUNWAY CLOSED MARKERS ARE NOT REQUIRED FOR THIS PROJECT. [2.18.2.1.2, 2.18.3]

ACCESS ROUTES - MARKING AND SIGNAGE

50. THE CONTRACTOR SHALL COORDINATE HAUL ROUTES, CLOSURES AND SCHEDULES WITH OTHER PROJECTS WHICH MAY BE UNDERWAY DURING THE SAME TIME PERIOD AS THIS CONTRACT. [2.18.4.2]

51. AIRCRAFT SHALL HAVE RIGHT-OF-WAY AT ALL TIMES. THE CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL YIELD TO AIRCRAFT AT ALL TIMES. CARE SHALL BE TAKEN NOT TO ALLOW ANY PERSONS TO ENTER AREAS WHERE THEIR IS POTENTIAL FOR AIRCRAFT BLAST. [2.18.4.2, 2.20.1]

HAZARD MARKING, LIGHTING AND SIGNAGE

52. LOW PROFILE BARRICADES SHALL BE PLACED AS SHOWN ON THE PLANS. CONTRACTOR TO MAINTAIN BARRICADES AT ALL TIMES. [2.20.1, 2.20.2.1, 2.20.2.5]

53. LOW PROFILE BARRICADES SHALL BE EQUIPPED WITH RED FLASHING LIGHTS. LIGHTS MUST MEET THE LUMINANCE REQUIREMENTS OF THE MDOT HIGHWAY DEPARTMENT. LIGHTS MUST BE OPERATED BETWEEN SUNSET AND SUNRISE AND DURING PERIODS OF LOW VISIBILITY. [2.20.2.1, 2.20.2.2]

54. THE CONTRACTOR'S EQUIPMENT STORAGE AREA SHALL BE AT THE LOCATIONS SHOWN. THE CONTRACTOR SHALL CONFINE THE STORAGE OF EQUIPMENT AND MATERIALS TO THIS AREA. THE MAXIMUM STORAGE HEIGHT SHALL BE AS INDICATED. ALL NON-PAVED AREAS THAT ARE DISTURBED BY CONTRACTOR'S OPERATIONS, SHALL BE GRADED, RESTORED WITH 4" OF TOPSOIL, SEEDDED AND MULCHED. SEED, TOPSOIL, AND MULCH SHALL BE IN ACCORDANCE WITH SPECIFICATIONS T-901, T905 AND T-908 RESPECTIVELY. DISTURBED AREAS SHALL BE RE-SEEDDED AND RESTORED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.

55. ALL AUTHORIZED VEHICLES AND EQUIPMENT REQUIRED TO WORK INSIDE SECURE AREAS SHALL HAVE A SIGN ON BOTH SIDES CONTAINING THE CONTRACTOR'S COMPANY NAME. PRIVATE VEHICLES OF THE CONTRACTOR'S PERSONNEL WILL NOT BE ALLOWED WITHIN THE SECURITY AREA. CONTRACTOR SHALL MARK AND LIGHT ALL VEHICLES IN THE WORK AREAS PER AC150/5210-5. [2.9.4.2]

WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION

56. LIGHTING EQUIPMENT MUST ADEQUATELY ILLUMINATE THE WORK AREA IF THE CONSTRUCTION IS TO BE PERFORMED DURING NIGHTTIME HOURS. REFER TO AC 150/5370-10 FOR MINIMUM ILLUMINATION LEVELS FOR NIGHTTIME PAVING PROJECTS. ADDITIONALLY, IT IS RECOMMENDED THAT ALL SUPPORT EQUIPMENT, EXCEPT HAUL TRUCKS, BE EQUIPPED WITH ARTIFICIAL ILLUMINATION TO SAFELY ILLUMINATE THE AREA IMMEDIATELY SURROUNDING THEIR WORK AREAS. THE LIGHTS SHOULD BE POSITIONED TO PROVIDE THE MOST NATURAL COLOR ILLUMINATION AND CONTRAST WITH A MINIMUM OF SHADOWS. THE SPACING MUST BE DETERMINED BY TRIAL. LIGHT TOWERS SHOULD BE POSITIONED AND ADJUSTED TO AIM AWAY FROM ATCT CABS AND ACTIVE RUNWAYS TO PREVENT BLINDING EFFECTS. SHIELDING MAY BE NECESSARY. LIGHT TOWERS SHOULD BE REMOVED FROM THE CONSTRUCTION SITE WHEN THE AREA IS REOPENED TO AIRCRAFT OPERATIONS. CONSTRUCTION LIGHTING UNITS SHOULD BE IDENTIFIED AND GENERALLY LOCATED ON THE CONSTRUCTION PHASING PLANS IN RELATIONSHIP TO THE ATCT AND ACTIVE RUNWAYS AND TAXIWAYS. SHOULD LIGHT TOWERS BE REQUIRED TO COMPLETE WORK, THE CONTRACTOR SHALL SUBMIT A LIGHTING PLAN TO THE RPR FOR REVIEW AND APPROVAL. [2.21]

PROTECTION RUNWAY AND TAXIWAY AREAS, ZONES AND SURFACES

57. WORK PERFORMED WITHIN THE RUNWAY SAFETY AREA (RSA) OR TAXIWAY SAFETY AREA (TSA) WILL REQUIRE CLOSURES OF THE RUNWAYS AND/OR TAXIWAYS AFFECTED AS INDICATED ON THE PLAN. ONLY THE AIRPORT CAN OPEN OR CLOSE AN AREA. THIS PROJECT DOES NOT INCLUDE WORK WITHIN AN RSA OR TSA.

58. NO WORK MAY BE PERFORMED WITHIN A RUNWAY SAFETY AREA (RSA), RUNWAY OBJECT FREE AREA (ROFA), TAXIWAY SAFETY AREA (TSA), TAXIWAY OBJECT FREE AREA (TOFA), OBSTACLE FREE ZONE (OFZ) OR APPROACH AND DEPARTURE SURFACES WHILE OPEN FOR AIRCRAFT OPERATIONS. CONTRACTOR SHALL NOT BEGIN WORK IN THE ASSOCIATED SAFETY AREA UNTIL THE RUNWAY OR TAXIWAY HAS BEEN CLOSED AND BARRICADES AND/OR CLOSED MARKERS HAVE BEEN PLACED. [2.22.1.1, 2.22.3.2, 2.22.3, 2.22.4]

59. THIS PROJECT WILL NOT IMPACT RUNWAY OR TAXIWAY SAFETY AREAS. EXISTING ACTIVE AIRCRAFT

AREAS WILL BE CLOSED OR MODIFIED IN ORDER TO ESTABLISH STANDARD SAFETY AREA DIMENSIONS BETWEEN CONSTRUCTION TRAFFIC AND AIRCRAFT OPERATIONS. DETAILS REGARDING IMPACTS TO OPERATING AREAS AND MEANS OF SEPARATING CONSTRUCTION FROM ACTIVE AIRCRAFT AREAS ARE SHOWN FOR EACH PHASE.

60. RUNWAYS AND TAXIWAYS WILL NOT RE-OPEN UNTIL ALL TRENCHES AND EXCAVATIONS WITHIN THE SAFETY AREAS AND OBJECT FREE AREAS HAVE BEEN BACKFILLED SUCH THAT NO SLOPE EXCEEDS FIVE PERCENT (5%) OR DROP EXCEEDS THREE (3) INCHES, PER MICHIGAN DEPARTMENT OF TRANSPORTATION (MDOT) AND FEDERAL AVIATION ADMINISTRATION (FAA) GRADING STANDARDS AND SPECIFICATIONS. [2.22.1.4, 2.22.3.5]

61. CONTRACTOR MUST PROMINENTLY MARK OPEN TRENCHES AND EXCAVATIONS AT THE CONSTRUCTION SITE WITH RED OR ORANGE FLAGS, AS APPROVED BY THE AIRPORT, AND LIGHT THEM WITH RED LIGHTS DURING HOURS OF RESTRICTED VISIBILITY OR DARKNESS. [2.22.1.4]

62. CONTRACTOR SHALL REMOVE ALL EQUIPMENT FROM THE ROFA WHEN NOT IN USE. [2.22.2]

63. CONTRACTOR SHALL NOT CROSS, PROTRUDE, ENTER OR BREAK THE PLANE OF THE OBJECT FREE ZONE (OFZ) OR THRESHOLD SURFACES WITH PERSONNEL, EQUIPMENT AND/OR MATERIAL. [2.22.4.3.6]

OTHER LIMITATIONS ON CONSTRUCTION

64. EQUIPMENT LOCATED WITHIN CONSTRUCTION AREAS SHALL NOT EXCEED THE HEIGHTS LISTED IN THE PROJECT COORDINATE TABLES.

65. CONTRACTOR SHALL NOT USE OPEN FLAME WELDERS OR TORCHES UNLESS ADEQUATE FIRE SAFETY PRECAUTIONS ARE PROVIDED AND THE AIRPORT HAS APPROVED THEIR USE. USE OF OPEN FLAME WELDERS OR TORCHES SHALL BE IN ACCORDANCE WITH THE CONTRACTOR'S CORPORATE POLICY AND PROJECT SAFETY PLAN. USE SHALL BE LIMITED TO AREAS WITH FIRE PROTECTION, AND CONTRACTOR SHALL PREVENT FIRES FROM STARTING AND LIMIT THE SPREAD IF ONE IS STARTED. [2.23.1.2]

66. THE USE OF ELECTRICAL BLASTING CAPS ARE PROHIBITED ON OR WITHIN 1,000 FT OF THE AIRPORT PROPERTY. [2.23.1.3]

67. ALL AREAS WITHIN THE AOA AND BUILDING ARE NON-SMOKING AREAS.

DEFINITIONS

- AC - ADVISORY CIRCULAR
AOA - AIR OPERATIONS AREA
CFR - CODE OF FEDERAL REGULATIONS
FAA - FEDERAL AVIATION ADMINISTRATION
FOD - FOREIGN OBJECT DEBRIS
AIRPORT = GERALD R. FORD INTERNATIONAL AIRPORT (GRR)
SPONSOR/OWNER = GERALD R. FORD INTERNATIONAL AIRPORT AUTHORITY (GFIAA)
NAVAID - NAVIGATIONAL AID
NOTAM - NOTICE TO AIRMEN
TSA - TRANSPORTATION SECURITY ADMINISTRATION

Table with 4 columns: NO., REVISIONS, BY, DATE. Row 1: 1, ADDENDUM No.1, C.M.S., 11.27.24.

Table with 2 columns: DRAWN, CHECKED. Row 1: STAFF, B.J.M.

GERALD R. FORD INTERNATIONAL AIRPORT
KENT COUNTY, MICHIGAN
RUNWAY 8R WILDLIFE HABITAT MITIGATION
SAFETY AND OPERATIONS NOTES

PROJECT NO. 2230912
SHEET NO. G-350